

Attachment B

Independent Design Peer Review

CROWN STREET, SURRY HILLS DEVONSHIRE ST TO OXFORD ST UPGRADE PEER REVIEW

CITY OF SYDNEY
MARCH 2023
FINAL

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Executive summary

Crown Street is an important place at the heart of the Surry Hills residential and business community—a place where the dynamic mixture of strolling, sitting, dining, and bicycle and vehicle movement all contribute to its energy and unique character.

The upgrade of Crown Street between Devonshire Street and Oxford Street therefore provides an opportunity to express the individuality and unique business offer of Crown Street, and contribute to a more social, lively and colourful streetscape.

The local residential and business community is best placed to bring the unique personality of Crown Street to life. Therefore, the role of the City's street upgrade should be to “set the stage” for the street's place identity to organically develop over time and in response to how residents and businesses carry out their day-to-day lives. The physical arrangement of street space, combined with the City's public domain palette, should provide the appropriate basis from which this organic evolution of place can occur.

The concept design presented in this peer review therefore seeks to:

- Maximise flexible use of the street to allow residents and businesses to express individuality of the street, and allow the public domain to respond to changing uses in adjacent buildings over time
- Design the public domain to maximise flexibility and efficiency, therefore minimising encroachment into existing car parking spaces
- Provide for continuous paths of travel for pedestrians
- Provide 'activation areas' for outdoor dining and the display of goods by businesses
- Improve the amenity of the public domain by maximising garden planting and street trees
- Improve amenity and safety of the street by recommending a reduction in travel speed to 30km/h.

Introduction

Project background

The City of Sydney plans to upgrade Crown Street in Surry Hills between Oxford Street and Devonshire Street, for a distance of about one kilometre. The intent of the upgrade is to improve walking in the area and help increase passing trade for local businesses.

A concept design was prepared by AECOM on behalf of the City of Sydney and released for public exhibition and comment between 8 August 2022 and 19 September 2022.

Street improvements proposed within this concept design include:

- removing timber electrical poles and placing overhead electrical cables underground
- new multifunction smartpoles with LED streetlights
- installing new footpath where there is currently pop-up alfresco dining
- footpath widening, kerb upgrades and pedestrian ramp upgrades
- replacing the existing signal crossing at Crown Street Public School with a raised zebra crossing
- new garden beds along the street, new bubblers, bike racks and seating.

Feedback received from the community and local businesses was generally supportive of the proposed upgrade, however a number of concerns have been raised with Council. Key issues raised in response to the current concept design include:

- Loss of parking on Crown Street
- Lack of individuality and concerns about Crown Street becoming too similar to other City of Sydney high streets.

Peer review process

Following receipt of community feedback on the current concept design, the City of Sydney undertook a peer review process with the engagement of ASPECT Studios. This process commenced with a series of site visits by the ASPECT Studios team, including site visits with the City of Sydney project team and Lord Mayor Clover Moore.

This was followed by a period of desktop review and design development, which culminated in a design workshop held at City of Sydney offices with Council's project team and representatives from AECOM and Sydney Civil. Feedback obtained during this workshop has informed the further development of the peer review concept design presented herein.

Draft issues of the report have been provided to Council and AECOM for review and comment, and suggested amendments incorporated into the final version of the report.

Purpose of this report

This report has been prepared by ASPECT Studios on behalf of the City of Sydney. It presents the findings and recommendations of the peer review process.

The report summarises the strategic design directions adopted for the review of the current concept design, and illustrates the proposed 'peer review concept design' for Council consideration.



Background policies and guidelines



Outdoor Dining Guidelines

City of Sydney June 2022

The City of Sydney is committed to supporting local businesses by encouraging seating for outdoor dining, creating a more active and inviting city street. The *Outdoor Dining Guidelines* provide the minimum requirements for operators to apply for an outdoor dining license.

Outdoor dining areas should provide a safe and enjoyable dining experience for customers, while keeping footways safe and accessible for all pedestrians.

This peer review has considered the design requirements included within the Guidelines, including:

- 1m minimum outdoor dining width
- 2m minimum corner clearance from a building corner
- 2m minimum clear pedestrian paths of travel
- 1m minimum clearance from bus stops or other service items
- 0.6m minimum clearance from parking spaces

The concept design presented herein provides allowances for outdoor dining, in some cases greater than the minimum requirements in the Guidelines.



Display of Goods on the Footway Local Approvals Policy

City of Sydney February 2021

The *Display of Goods on the Footway Local Approvals Policy* sets the circumstances and conditions under which the City of Sydney will allow goods to be displayed on local footways.

The City of Sydney encourages the activation of footways through the public display of goods, providing that operators comply with the Policy. The Policy provides the minimum requirements for Council to justify the approval for the public display of goods on city streets. Some of these requirements include:

This peer review has considered the design requirements included within the Policy, including:

- kerbside displays with the provision that outdoor dining is already located towards the kerb
- 2m minimum clear pedestrian paths of travel
- 2m minimum corner clearance from a building corner
- 0.6m minimum setback from kerb
- 1m minimum clearance from pedestrian a signalised crossing
- 0.6m maximum depth of display

The concept design presented herein provides allowances for the display of goods on the footway should operators choose to. In some cases, the concept plans provide allowances in excess of the minimum requirements of the Policy.

Responding to the community

Peer review aim

The aim of the peer review was to review and comment on the current concept design for Crown Street, with a particular focus on addressing areas of community concern and highlighting opportunities for:

- Minimising the reduction in existing parking spaces and increasing provision of parking where possible
- Bringing a unique personality to Crown Street.

Responding to place identity

Like many high streets in Sydney, Crown Street, Surry Hills is an important place at the heart of its residential and business community.

The upgrade of Crown Street between Devonshire Street and Oxford Street provides an opportunity to reinvigorate this key public space while building upon its unique existing characteristics.

Setting the stage

The local residential and business community is best placed to bring the unique personality of Crown Street to life. The role of the street upgrade project should therefore be to "set the stage" for Crown Street's place identity to organically develop over time and in response to how residents and commercial tenants carry out their day-to-day life and work on the street. The City's public domain palette—its suite of paving, furniture, and fixtures—provides the appropriate basis from which this organic evolution of place identity can occur.

The concept design should therefore seek to use the City palette in a way that maximises opportunities for public use of the street—through strolling, dwelling, seating, and the like—as well as business activation through outdoor dining, displaying goods, and potential areas for future activation and/or installations.

Street activation

The City has recently updated its Display of Goods on the Footway Local Approval Policy and Outdoor Dining Policy.

A primary aim of the City's policies is to support business and street activation as well as ensure safe, dignified, and equitable access to pedestrians and other users travelling along the street.

Both outdoor dining and display of goods provide opportunity to express the individuality and unique business offer of Crown Street, and contribute to a more social, lively and colourful streetscape.

The concept design provides opportunities for use and activation of the street by businesses through well-defined and wider areas of footpath.

The display of goods located either along shop frontage or kerb side outside their premises in accordance with the policy can be an opportunity to express the individuality of the business mix that forms the character of Crown Street.

Similarly for outdoor dining the choice of furniture, including tables, chairs, and umbrellas can reflect and build on the existing character of Surry Hills and as well express the individuality of these businesses

Design strategies

The peer review process has culminated in a proposed update to the concept design. This updated 'peer review concept design' is presented within the following pages of this report.

The updated concept design has been informed by a series of design strategies, discussed below.

Design Strategy 1 - Embed flexibility

New areas of public domain—such as footpath and planting areas that are gained with the introduction of new kerb extensions—should maximise flexibility of use for the variety of local businesses along Crown Street.

Flexibility should also be improved for existing areas of footpath that provide an opportunity to better relate and respond to the properties they are located in front of.

Local businesses along Crown Street have varying needs, while tenancies also turnover over time. This warrants a flexible design of the Crown Street verge.

Design Strategy 2 - Rationalise kerb-side parking

Locations where new kerb extensions are introduced should be reviewed and, where appropriate, adjusted to reduce impacts to the existing provision of parking supply on Crown Street.

Loading zones, timed parking with special provisions, taxi zones, and the like should also be reviewed to ensure parking type responds to the requirements of Crown Street at different times of day and night.

Design Strategy 3 - Re-locate car share

While car share programs are critical to reducing the need for car ownership, car share parking spaces need not be located on a destination high street such as Crown Street.

Existing car share parking spaces on Crown Street can be relocated to nearby side streets without impacting convenience of location of car share users, while providing a more appropriate residential street setting for car share users to access vehicles, and load and unload personal belongings.

Design Strategy 4 - Greening

Kerbside planting can be extended along extended sections of kerbside parking that would otherwise provide little relief from the extensive hardscape of the street environment.

The extent of planting can also be varied along the length of Crown Street to respond to its varying rhythm. For example, abundant planting beds can be installed in quieter sections of the street, while smaller, more contained planting beds can be provided in areas of higher pedestrian traffic and vibrancy.

Design Strategy 5 - Street trees succession

Increasing tree canopy with additional street tree planting is an important part of the proposed upgrade. However, the existing Plane trees on Crown Street have drawn negative comments from the community over the years, and the upgrade works provides an opportunity to plan for their eventual replacement.

New succession street tree planting should be promoted in locations that provide tree canopy in potential future locations of Plane tree removal.

Design Strategy 6 - Reduce speed

Slow streets are the heart of any successful neighbourhood or town centre. 30km/h speed limits where people and traffic mix result in streets that are safe, healthy, green, and liveable—streets for public life.

Crown Street is the ideal candidate for a speed reduction to 30km/h and an opportunity to prioritise place outcomes for the Surry Hills neighbourhood over private vehicle traffic driving through it.

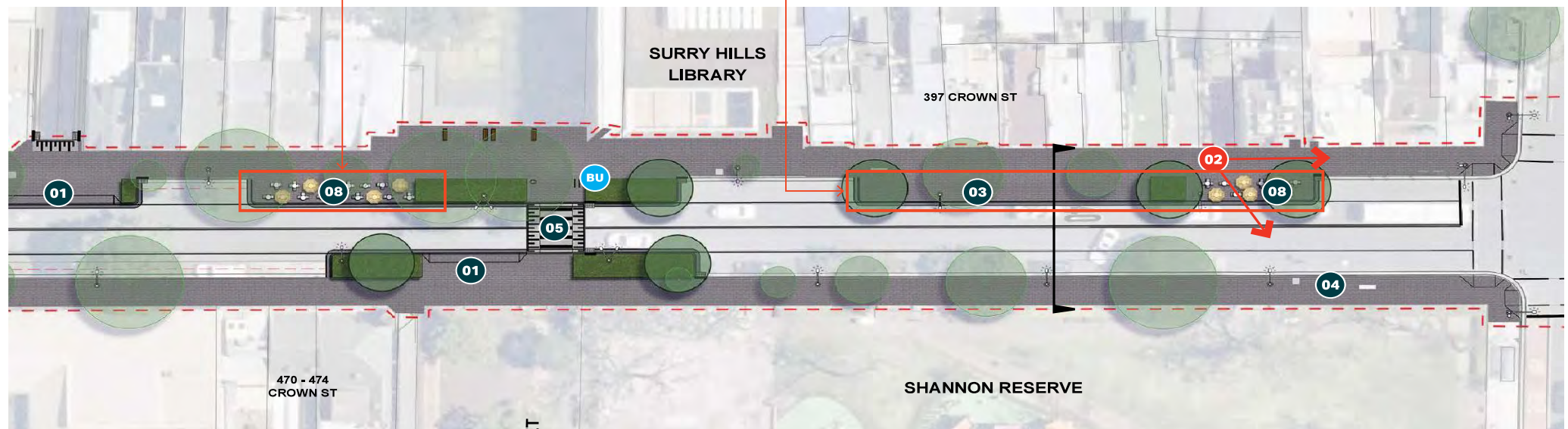
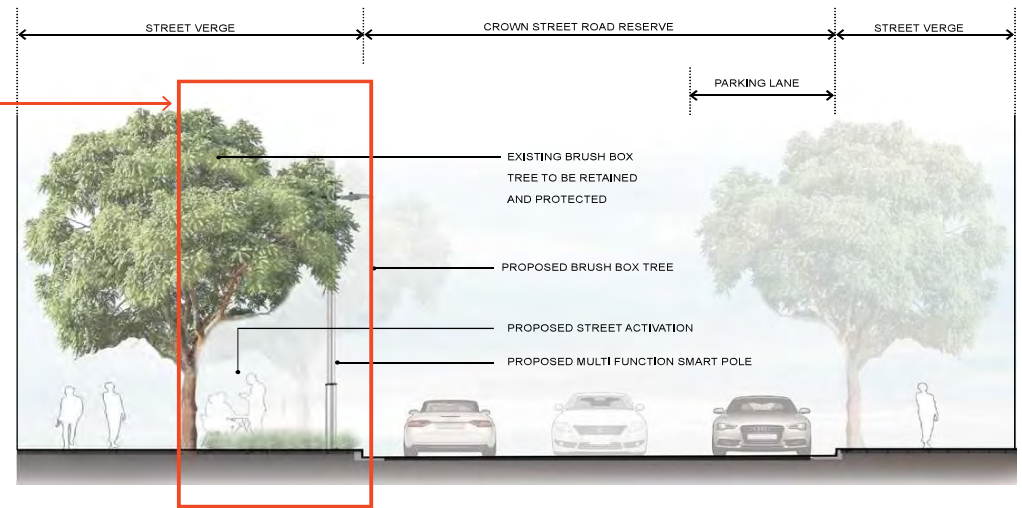


Review of existing concept design

The current concept design proposes a significant gain in public space within the verge - the result of kerb extensions and lane narrowing along much of Crown Street.

The resulting public domain design proposes a binary approach to the design of this additional public space, creating large areas of outdoor dining interspersed with large areas of planting, both of which occupy the full width of the proposed kerb extension.

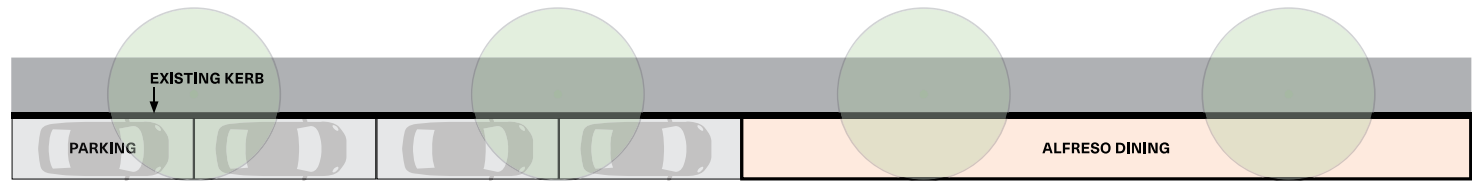
LARGE AREAS OF OUTDOOR DINING / PLANTING WITHIN NEW KERB EXTENSIONS



Proposed revised approach to verge

We believe there is an opportunity to adopt an alternate arrangement to public domain in the verge that:

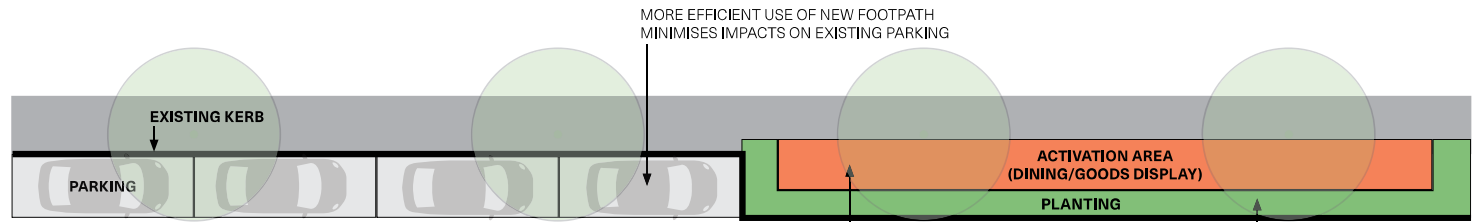
- Provides more opportunities for greening and tree planting
- Creates flexible areas of pedestrian paving—what we call Activation Areas—that can be used for outdoor dining, displaying goods, or other opportunities that the local and business community may develop over time
- Provides buffer planting between Activation Areas and moving vehicular traffic in adjacent travel lanes
- Provides an efficient use of verge space that allows for reducing impacts on existing car parking supply.



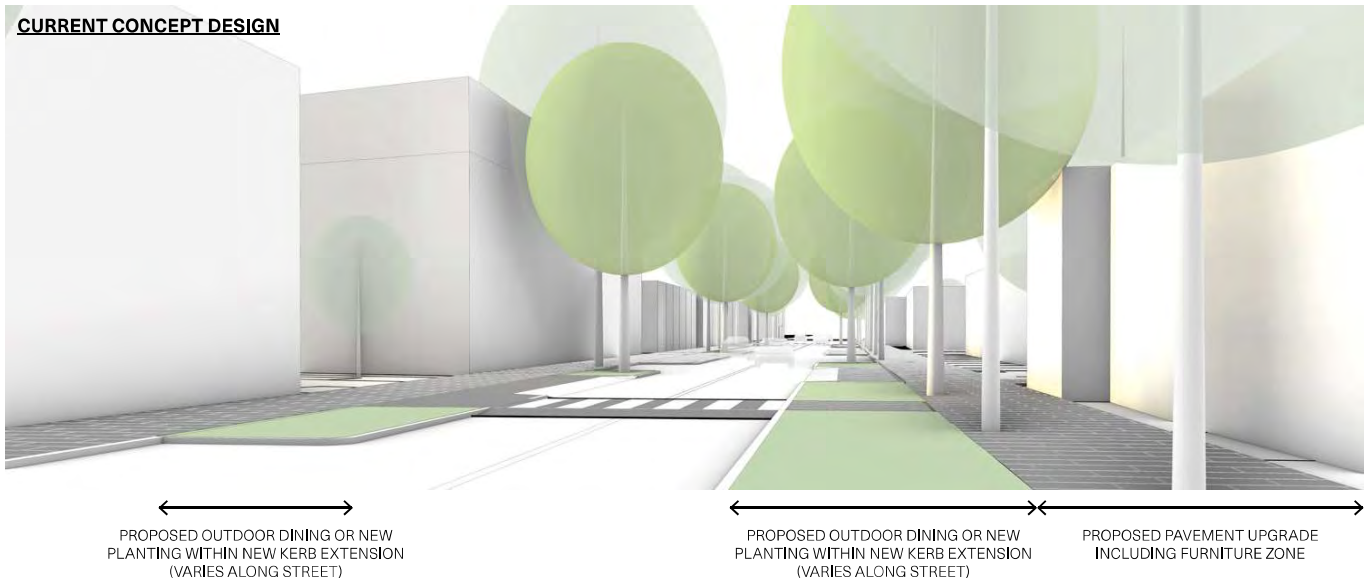
EXISTING



CURRENT CONCEPT DESIGN



PEER REVIEW CONCEPT



Typical verge proposals

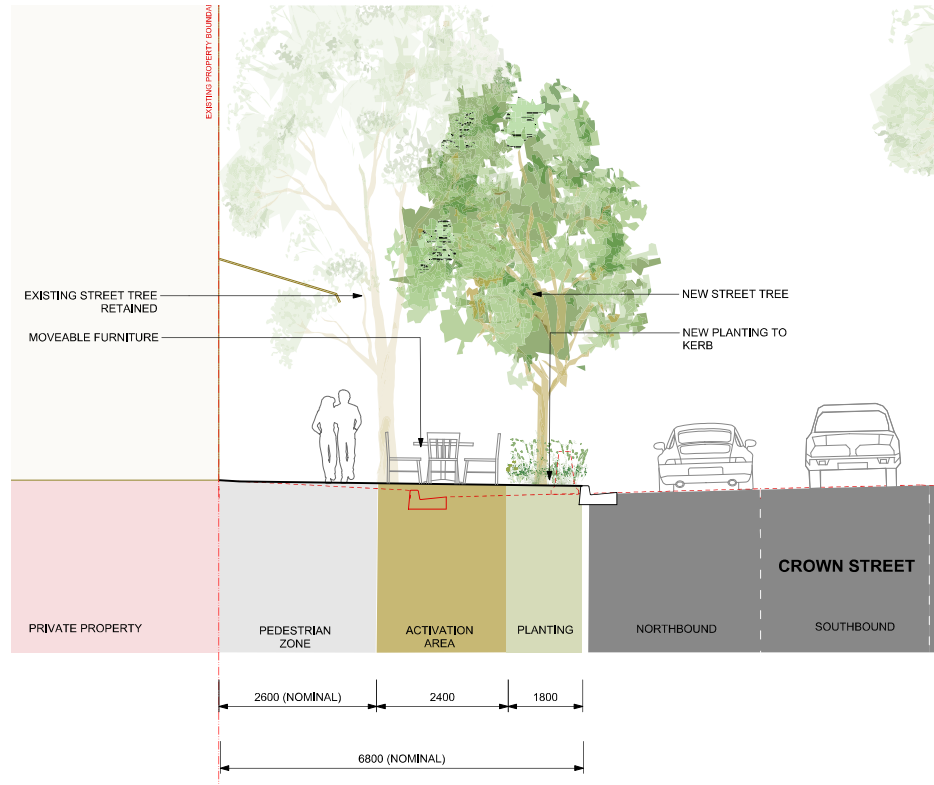
Busy areas

Cross sections shown here illustrate typical arrangements for new footpath paving (designated 'activation area') and planting in areas of kerb extensions.

Activation areas will provide a flexible approach to new public domain that allows for verge activation to respond to the varying needs of residents and local businesses along Crown Street.

When outside a food and beverage premise, activation areas can cater for outdoor dining. When located outside non-food and beverage premises, activation areas provide space for the display of outdoor goods or other activation opportunities subject to consultation between tenants and Council.

TYPICAL ARRANGEMENT TYPE 1 NO ACTIVATION ALONG BUILDING EDGE



Quiet areas

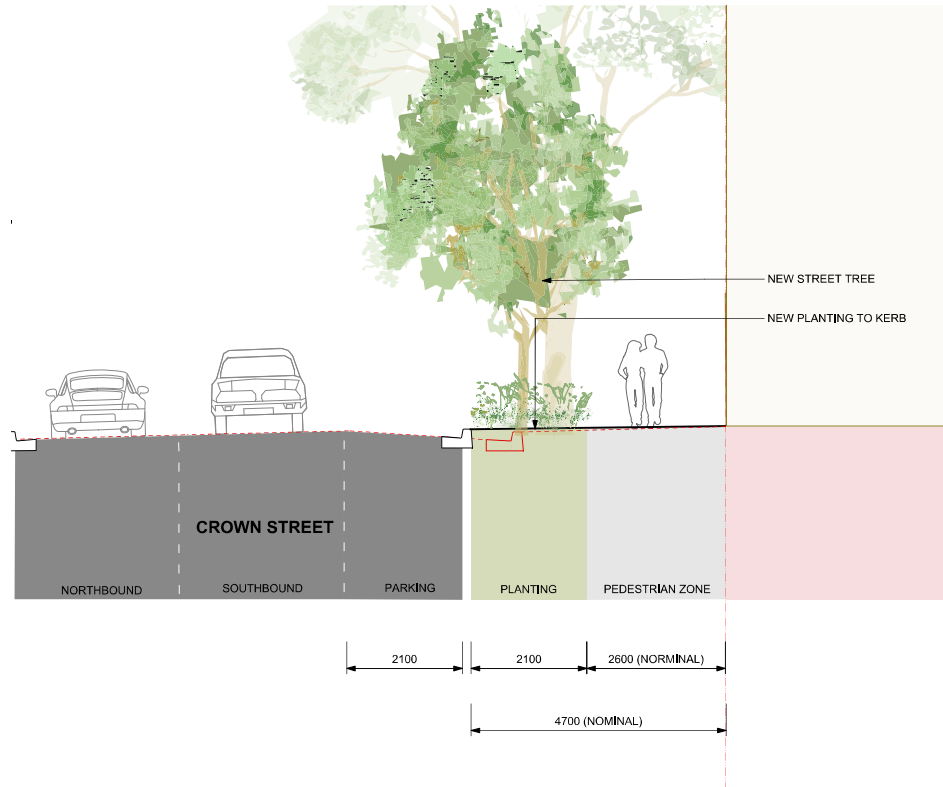
Cross sections shown here illustrate the typical approach to providing more planting beds in areas where car parking is retained—travel and parking lane width reductions allow for the realignment of the kerb and creation of additional verge space for planting.

Kerb realignments provided adjacent car parking spaces provide a green buffer between pedestrians and vehicles, improving the walking experience on Crown Street.

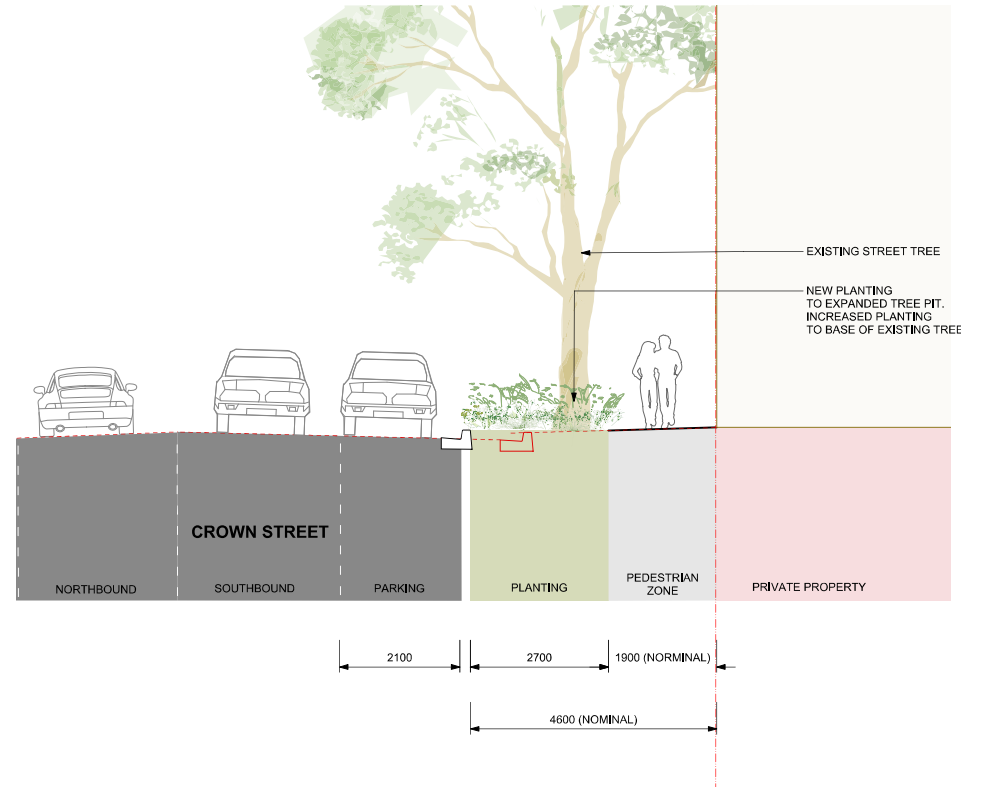
Around existing street trees, kerb realignment provides additional width where new planting and pedestrian paving is possible. A detailed assessment will need to be made at each existing tree location to determine if widening pedestrian pavement is possible in the context of existing tree roots.

While kerb realignments involved detailed investigation of levels and crossfalls on adjacent footpaths and travel/parking lanes, the introduction of planting along the kerb provides more flexibility in how any differences in levels are resolved. Rather than paved pathways through planting beds, stepping stones also provide a low cost and flexible approach to informal access through longer sections of planting.

**TYPICAL ARRANGEMENT TYPE 2
NEW TREES AND PLANTING ADJACENT PARKING**



**TYPICAL ARRANGEMENT TYPE 3
NEW PLANTING AROUND EXISTING LARGE TREES ADJACENT PARKING**



Proposed



Existing



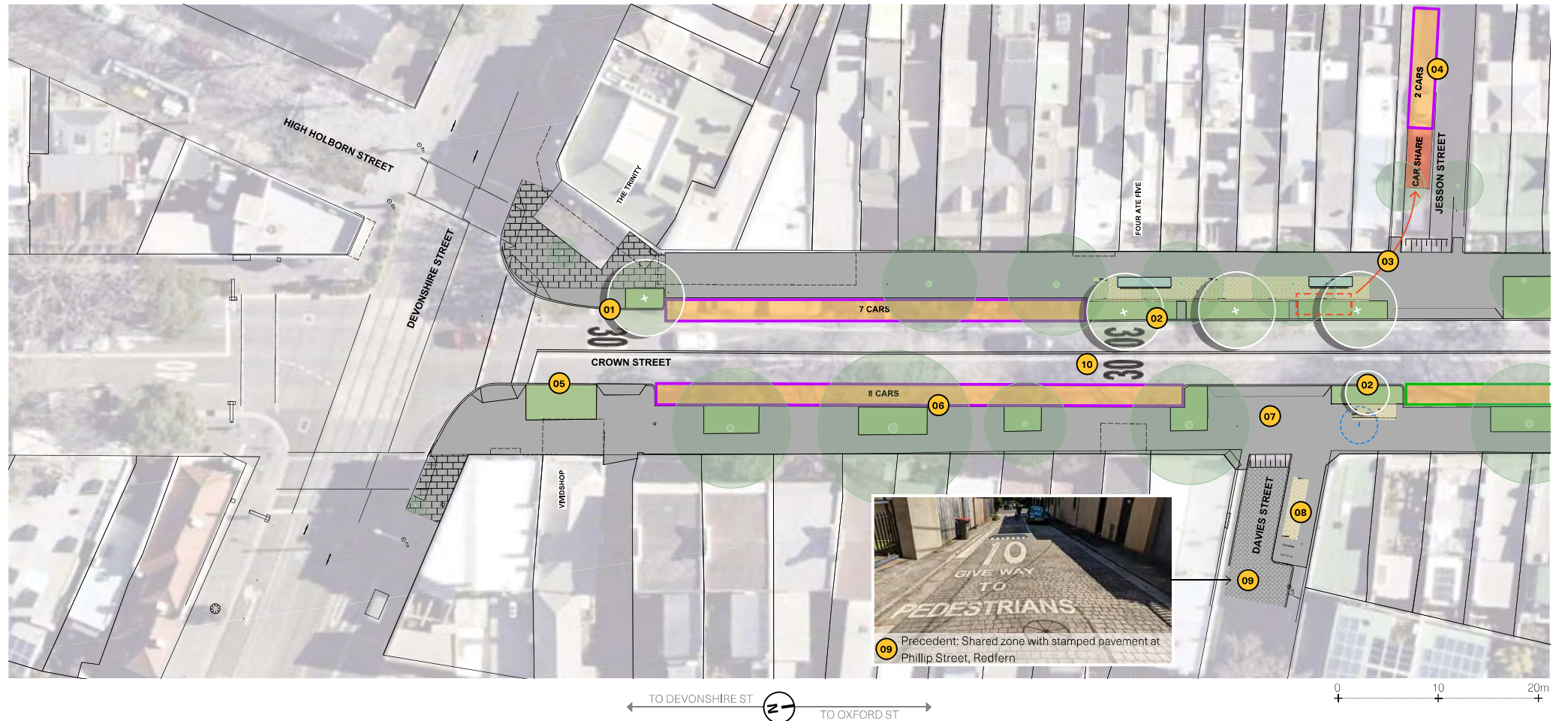
Peer review concept design

Crown Street Peer Review Plan 1 of 8

1. Kerb extension and new street tree as per AECOM concept design.
2. New kerb extension to create activation area with kerbside planting and additional street trees at existing commercial premises.
3. Relocate 1 no. existing car share parking space to Jesson St.
4. Convert 2-3 no. existing unmarked 2P parking spaces on Jesson St to 2 no. marked 2P parking spaces.
5. Kerb extension to provide additional pedestrian paving and planting at intersection, with operations to intersection to remain unchanged (subject to TfNSW approval).
6. Kerb alignment adjusted (to minimum compliant parking space width) to maximise footpath and planting space. Increased planting beds to base of existing trees.
7. Continuous footpath treatment and kerb extension at Davies St.
8. Kerb extension to Davies St to provide additional activation area and space for new bike parking.
9. New shared zone with stamped pavement to Davies St, to be consistent with other typical City of Sydney laneway shared zones (subject to further investigation and TfNSW approval).
10. Speed reduction to Crown Street to 30km/h (subject to TfNSW approval).

Legend

- | | | | |
|--|---|--|-------------------------------|
| | Upgraded Pedestrian Pavement | | Proposed Tree |
| | Existing Upgraded Pedestrian Pavement | | Existing Tree to be Retained |
| | Proposed Stamped Pavement | | Existing Tree to be Relocated |
| | Existing Brick Paving Retained | Parking approach assessed on existing numbers | |
| | Proposed Activation Areas | | Parking Balanced |
| | Existing Dining Zone to be Retained | | Parking Increased |
| | Existing Dining Zone to be Relocated to Kerb Side | | Parking Decreased |
| | Landscape Planting | | Car Share Parking |
| | Bus Stop | | |

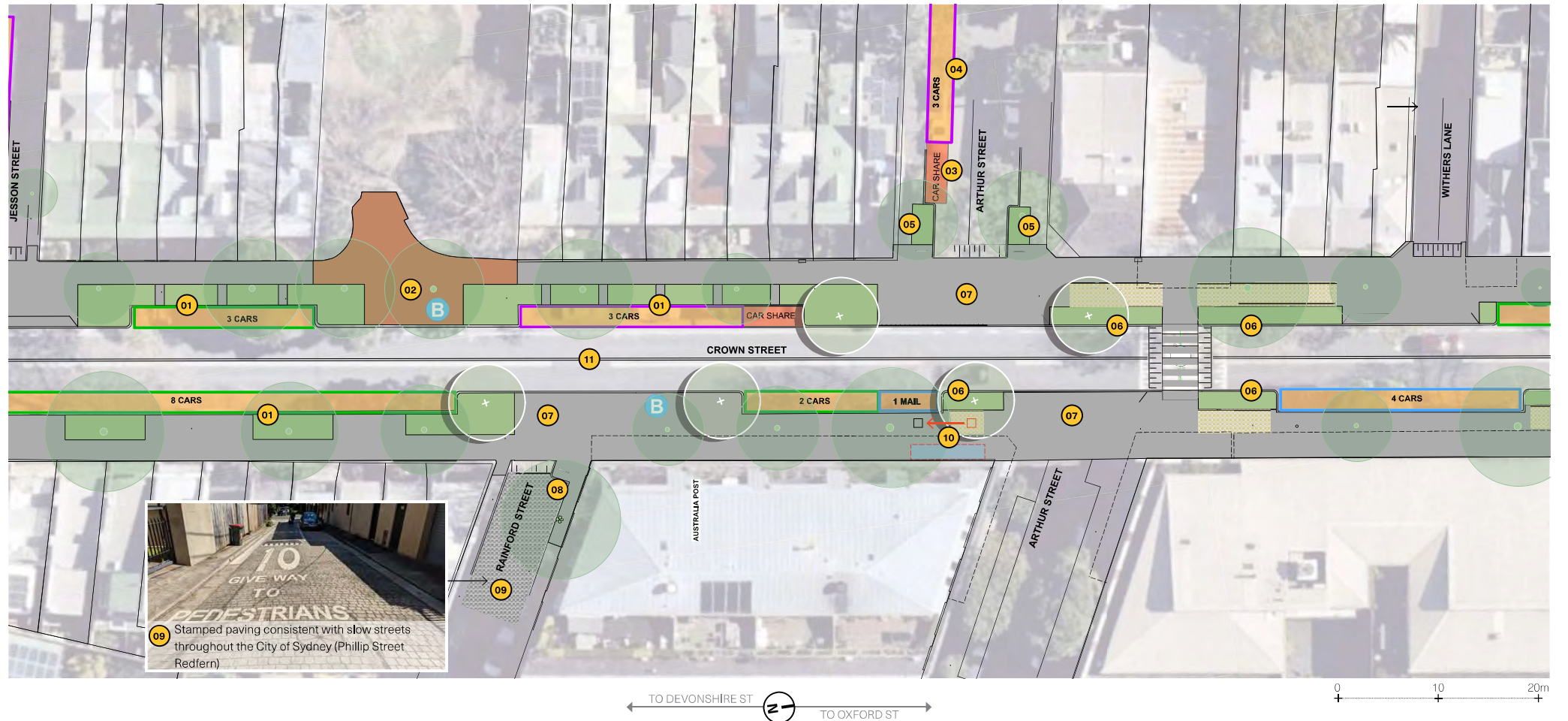


Crown Street Peer Review Plan 2 of 8

1. Kerb alignment adjusted (to minimum compliant parking space width) to maximise footpath and planting space. Continuous planting bed provided with intermittent pavements for access to parked vehicles.
2. Bus stop 'in the park' - in-lane bus stop located to align with Tudor St park, with existing brick paving to full width of Tudor St lot boundary to be retained.
3. Relocate 1 no. existing car share parking space from outside The Clock Hotel to Arthur St.
4. Convert 4 no. existing unmarked 2P parking spaces on Arthur St to 3 no. marked 2P parking spaces.
5. Kerb extensions to Arthur St to provide additional space for planting and bike parking, and lane narrowing for traffic calming.
6. New kerb extension to create activation area with kerbside planting, and additional street trees at existing commercial premises.
7. Continuous footpath treatment and kerb extension where side streets.
8. Kerb extension to Rainford St to provide additional space for new bike parking.
9. New shared zone with stamped pavement to Rainford St, to be consistent with other typical City of Sydney laneway shared zones (subject to further investigation and TfNSW approval).
10. Minor relocation of existing mail box to provide additional activation area.
11. Speed reduction to Crown Street to 30km/h (subject to TfNSW approval).

Legend

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|--|---|--|-------------------------------|
| | Upgraded Pedestrian Pavement | | Proposed Tree |
| | Existing Upgraded Pedestrian Pavement | | Existing Tree to be Retained |
| | Proposed Stamped Pavement | | Existing Tree to be Relocated |
| | Existing Brick Paving Retained | Parking approach assessed on existing numbers | |
| | Proposed Activation Areas | | Parking Balanced |
| | Existing Dining Zone to be Retained | | Parking Increased |
| | Existing Dining Zone to be Relocated to Kerb Side | | Parking Decreased |
| | Landscape Planting | | Car Share Parking |
| | Bus Stop | | |



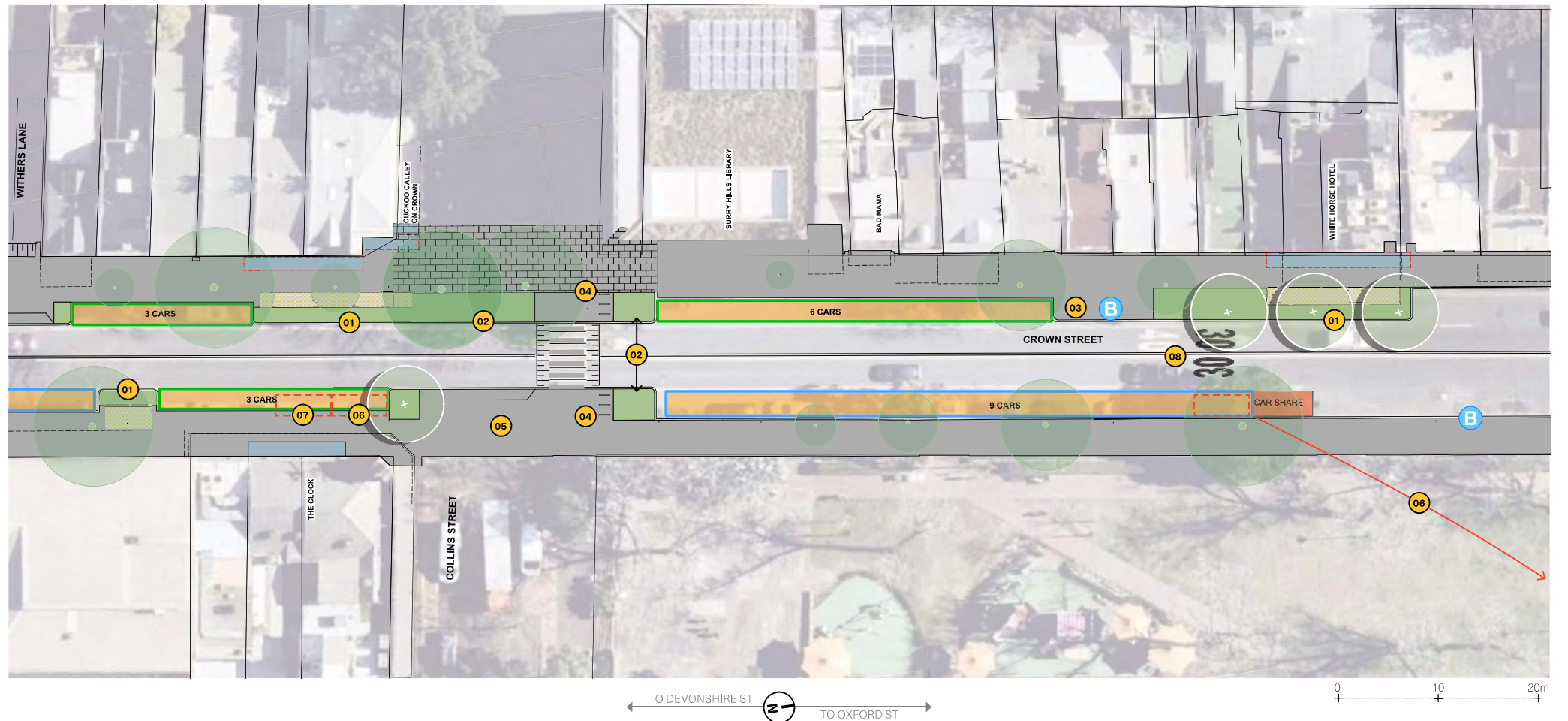
09 Stamped paving consistent with slow streets throughout the City of Sydney (Phillip Street Redfern)

Crown Street Peer Review Plan 3 of 8

1. New kerb extension to create activation area with kerbside planting, and additional street trees at existing commercial premises.
2. New kerb extensions with additional planting (assess amenity and performance of raingarden application in main street with potential to replace with street planting consistent with approved planting palette).
3. New kerb extension for in-lane bus stop, 9m paving zone shown (to be reviewed at detailed design stage to consider user access requirements).
4. New bike parking a key arrival point to Crown St and point of dismount for cyclists approach via Crown St or Collins St.
5. Continuous footpath treatment and kerb extension at Collins St.
6. Relocate 2 no. existing car share parking spaces to Richards Ln.
7. Relocate 1 no. existing car share parking spaces to Arthur St.
8. Speed reduction to Crown Street to 30km/h (subject to TfNSW approval).

Legend

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|--|---|--|-------------------------------|
| | Upgraded Pedestrian Pavement | | Proposed Tree |
| | Existing Upgraded Pedestrian Pavement | | Existing Tree to be Retained |
| | Proposed Stamped Pavement | | Existing Tree to be Relocated |
| | Existing Brick Paving Retained | Parking approach assessed on existing numbers | |
| | Proposed Activation Areas | | Parking Balanced |
| | Existing Dining Zone to be Retained | | Parking Increased |
| | Existing Dining Zone to be Relocated to Kerb Side | | Parking Decreased |
| | Landscape Planting | | Car Share Parking |
| | Bus Stop | | |

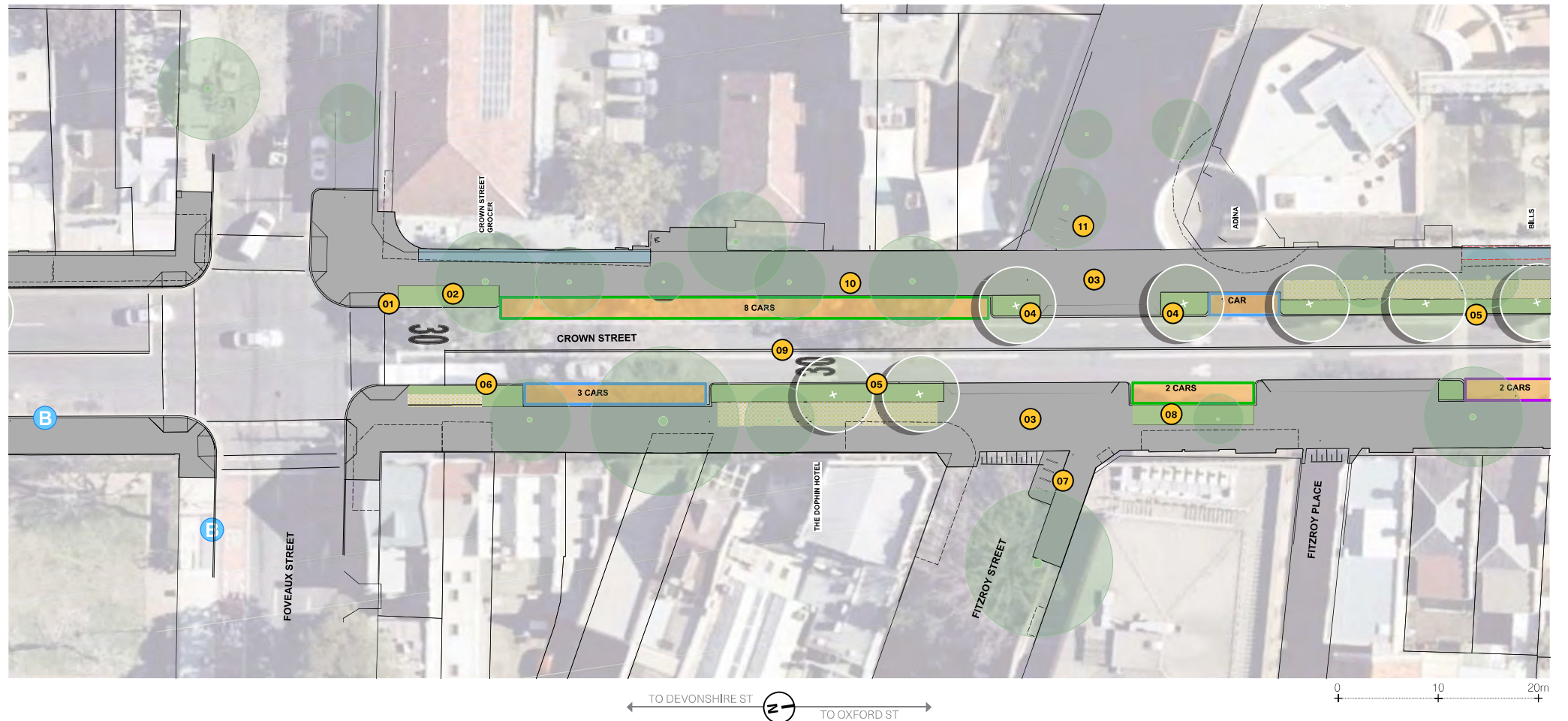


Crown Street Peer Review Plan 4 of 8

1. Kerb extension as per AECOM concept design.
2. Additional kerbside planting.
3. Continuous footpath treatment and kerb extension at Fitzroy St.
4. Kerb extension with new street tree planting.
5. New kerb extension to create activation area with kerbside planting and additional street trees at existing commercial premises.
6. New kerb extension to provide additional pedestrian paving for activation and planting. Adjustment to signal operations to be investigated (subject to TfNSW approval).
7. Kerb extension to Fitzroy Street to provide additional space for new bike parking.
8. Kerb alignment adjusted (to minimum compliant parking space width) to maximise footpath and planting space.
9. Speed reduction to Crown Street to 30km/h (subject to TfNSW approval).
10. Existing driveway removed as per current concept design.
11. Provide additional bike parking on Arthur St to offset removal/relocation of existing bike parking on Crown St.

Legend

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|--|---|--|-------------------------------|
| | Upgraded Pedestrian Pavement | | Proposed Tree |
| | Existing Upgraded Pedestrian Pavement | | Existing Tree to be Retained |
| | Proposed Stamped Pavement | | Existing Tree to be Relocated |
| | Existing Brick Paving Retained | Parking approach assessed on existing numbers | |
| | Proposed Activation Areas | | Parking Balanced |
| | Existing Dining Zone to be Retained | | Parking Increased |
| | Existing Dining Zone to be Relocated to Kerb Side | | Parking Decreased |
| | Landscape Planting | | Car Share Parking |
| | Bus Stop | | |

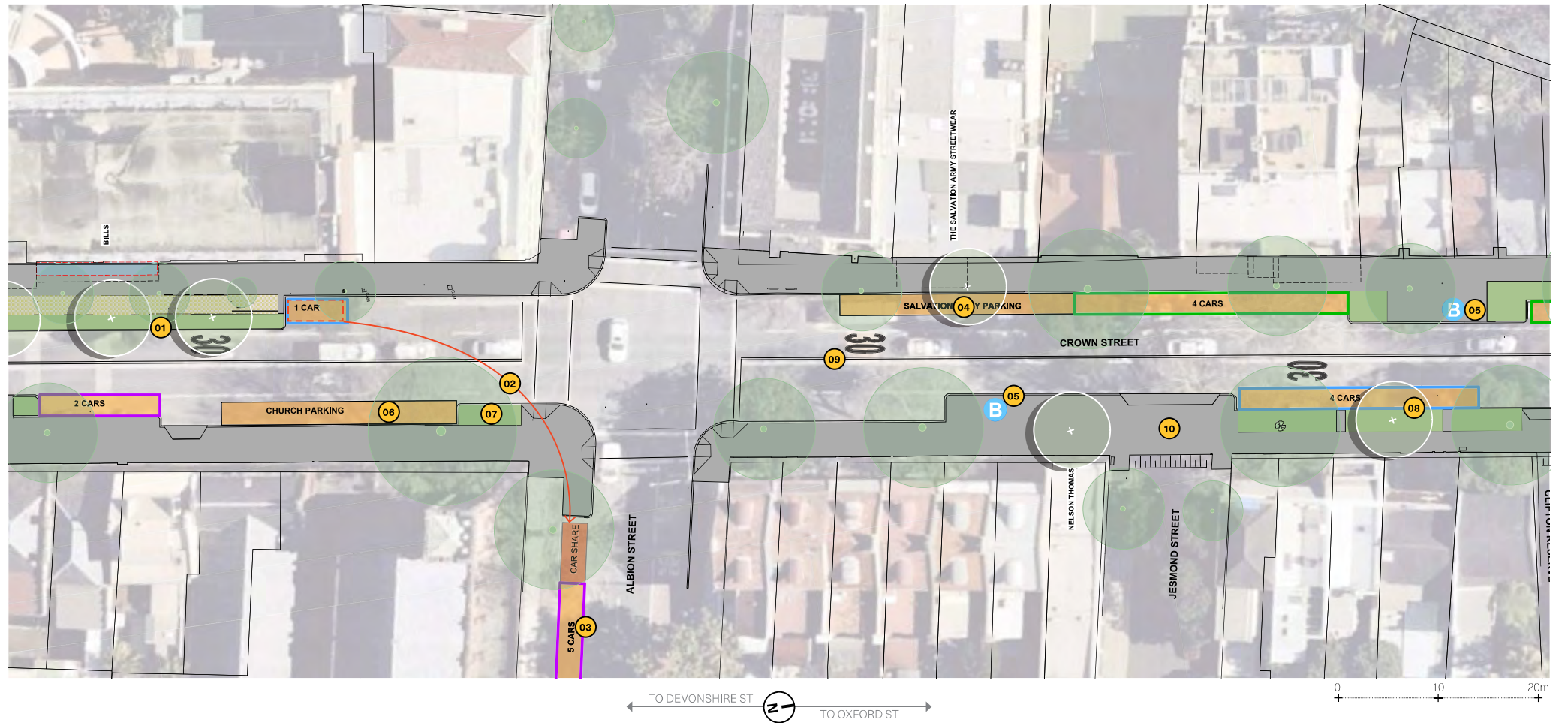


Crown Street Peer Review Plan 5 of 8

1. New kerb extension to create activation area with kerbside planting and additional street trees at existing commercial premises.
2. 1x car share relocated to Albion St, replaced with 2P parking space.
3. Convert 6 no. existing unmarked 2P parking spaces on Albion St to 5 no. marked 2P parking spaces.
4. Existing Salvation Army parking area. Investigate opportunity to introduce Loading Zone and/or after-hours general parking (subject to further consultation).
5. New kerb extension for in-lane bus stop, 9m paving zone shown, to be reviewed at detailed design stage to consider user access requirements (subject to approval by TfNSW).
6. Existing signed church parking area. Investigate opportunity to introduce timed after-hours general parking (subject to further consultation).
7. Kerb extension to provide additional pedestrian paving and planting at intersection.
8. Kerb alignment adjusted (to minimum compliant parking space width) to maximise footpath and planting space. Continuous planting bed provided with intermittent pavements for access to parked vehicles.
9. Speed reduction to Crown Street to 30km/h (subject to TfNSW approval).
10. Continuous footpath treatment and kerb extension at Jesmond St.

Legend

	Upgraded Pedestrian Pavement		Proposed Tree
	Existing Upgraded Pedestrian Pavement		Existing Tree to be Retained
	Proposed Stamped Pavement		Existing Tree to be Relocated
	Existing Brick Paving Retained	Parking approach assessed on existing numbers	
	Proposed Activation Areas		Parking Balanced
	Existing Dining Zone to be Retained		Parking Increased
	Existing Dining Zone to be Relocated to Kerb Side		Parking Decreased
	Landscape Planting		Car Share Parking
	Bus Stop		

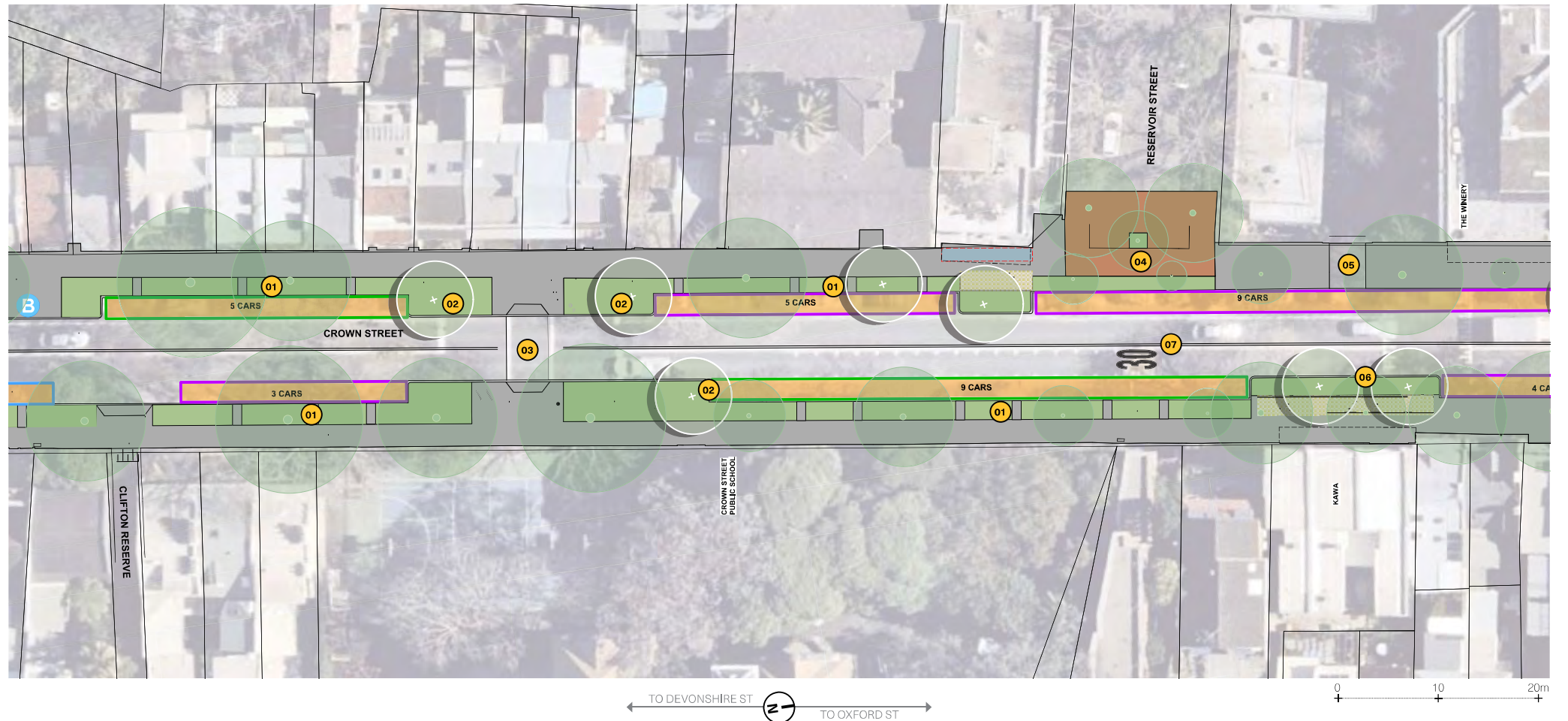


Crown Street Peer Review Plan 6 of 8

1. Kerb alignment adjusted (to minimum compliant parking space width) to maximise footpath and planting space. Continuous planting bed provided with intermittent pavements for access to parked vehicles.
2. New kerb extension with planting and street trees to provide additional canopy cover.
3. Existing signalised crossing retained in current location.
4. Existing brick paving to full width of Reservoir St lot boundary to be retained.
5. Existing heritage driveway retained.
6. New kerb extension to create activation area with kerbside planting and additional street trees at existing commercial premises.
7. Speed reduction to Crown Street to 30km/h (subject to TfNSW approval).

Legend

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|--|---|--|-------------------------------|
| | Upgraded Pedestrian Pavement | | Proposed Tree |
| | Existing Upgraded Pedestrian Pavement | | Existing Tree to be Retained |
| | Proposed Stamped Pavement | | Existing Tree to be Relocated |
| | Existing Brick Paving Retained | Parking approach assessed on existing numbers | |
| | Proposed Activation Areas | | Parking Balanced |
| | Existing Dining Zone to be Retained | | Parking Increased |
| | Existing Dining Zone to be Relocated to Kerb Side | | Parking Decreased |
| | Landscape Planting | | Car Share Parking |
| | Bus Stop | | |



Crown Street Peer Review Plan 7 of 8

1. New kerb extension for in-lane bus stop to alleviate constrained footpath space where bus stop furniture is currently installed. New planting bed and street tree provided within kerb extension.
2. Explore potential for traffic stop line relocation on Campbell St to allow for kerb radius reduction without impeding vehicle turning movements.
3. New street tree in tree pit.
4. New kerb extension to create activation area with kerbside planting and additional street tree at existing commercial premises.
5. Relocate 1 no. existing car share parking space to Goulburn St (refer to next sheet for location of new car share space).
6. Existing mail parking space retained.
7. New street tree in kerb extension.
8. Convert existing no parking area to 1 no. 5 minute P line marked parking space.
9. Speed reduction to Crown Street to 30km/h (subject to TfNSW approval).

Legend

- | | | | |
|--|---|--|-------------------------------|
| | Upgraded Pedestrian Pavement | | Proposed Tree |
| | Existing Upgraded Pedestrian Pavement | | Existing Tree to be Retained |
| | Proposed Stamped Pavement | | Existing Tree to be Relocated |
| | Existing Brick Paving Retained | Parking approach assessed on existing numbers | |
| | Proposed Activation Areas | | Parking Balanced |
| | Existing Dining Zone to be Retained | | Parking Increased |
| | Existing Dining Zone to be Relocated to Kerb Side | | Parking Decreased |
| | Landscape Planting | | Car Share Parking |
| | Bus Stop | | |



Crown Street Peer Review Plan 8 of 8

1. New kerb extension to create activation area with kerbside planting and additional street trees at existing commercial premises.
2. Relocate 1 no. existing car share parking space to Goulburn St.
3. Convert 13 no. existing unmarked 2-4P parking spaces on southern side of Goulburn St to 12 no. marked 2-4P parking spaces.
4. Transplant existing tree into planting bed provided by new kerb extension.
5. Existing planting retained.
6. New kerb extension as part of AECOM design.
7. Continuous footpath treatment and kerb extension at Little Oxford St.
8. Speed reduction to Crown Street to 30km/h (subject to TfNSW approval).

Legend

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|--|---|--|-------------------------------|
| | Upgraded Pedestrian Pavement | | Proposed Tree |
| | Existing Upgraded Pedestrian Pavement | | Existing Tree to be Retained |
| | Proposed Stamped Pavement | | Existing Tree to be Relocated |
| | Existing Brick Paving Retained | Parking approach assessed on existing numbers | |
| | Proposed Activation Areas | | Parking Balanced |
| | Existing Dining Zone to be Retained | | Parking Increased |
| | Existing Dining Zone to be Relocated to Kerb Side | | Parking Decreased |
| | Landscape Planting | | Car Share Parking |
| | Bus Stop | | |



Summary

The below presents a summary of the net change in provision of car parking spaces and tree planting between existing conditions, the existing concept design, and the peer review concept design presented herein.

Methodology

In reality, the number of car parking spaces available along any given length of kerb varies with vehicle size and parking behaviour.

Quantities of existing and proposed car parking spaces are therefore presented via two methods—referred to herein as the ‘Australian Standard method’ and ‘on-site observation method’. These methods provide what we have determined to be nominal minimum and maximum numbers of potential car parking spaces. The methods are described below:

1. Australian Standard method: Determines the lineal metre length of kerb in a given area of car parking and divides this by a typical 6m length car parking space to yield a total number of car parking spaces. This typically yields what can be considered a minimum car parking provision.

2. On-site observation method: Observes user behaviour on-site to determine to number of cars parking in any given area. This typically yields what can be considered a maximum car parking provision.

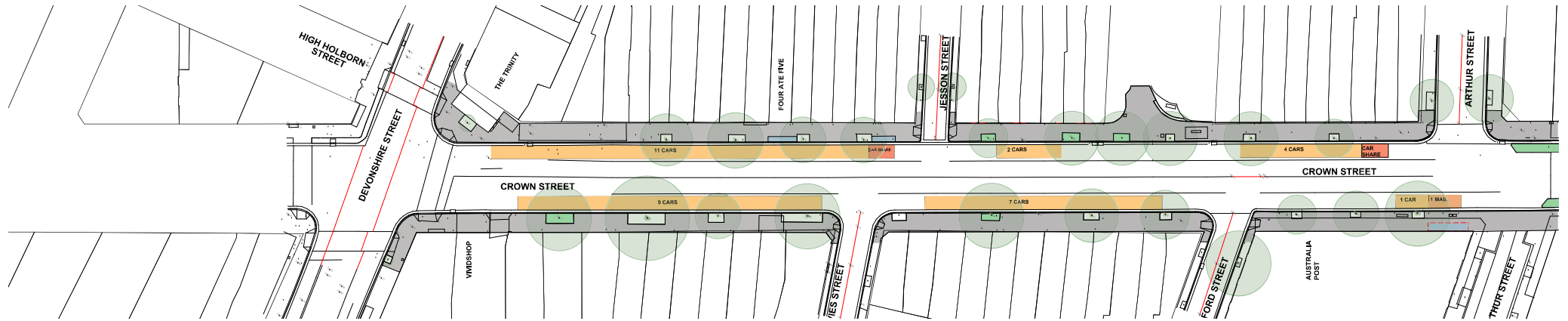
Quantities of existing trees were determined via walking the length of the project site and counting tree numbers.

TOTAL Devonshire St to Oxford St			
	Existing	Current concept	Peer review concept
Pre-Covid car parking spaces on Crown St (minimum per Australian Standard)	151	n/a	n/a
Car parking spaces replaced by alfresco dining	-27	n/a	n/a
Car parking spaces on Crown St (minimum per Australian Standard)	124	116	134
Car share spaces relocated from Crown St to side streets	n/a	0	6
Reduction in car parking spaces on side streets	n/a	0	-6
Street trees on Crown St	96	132	136

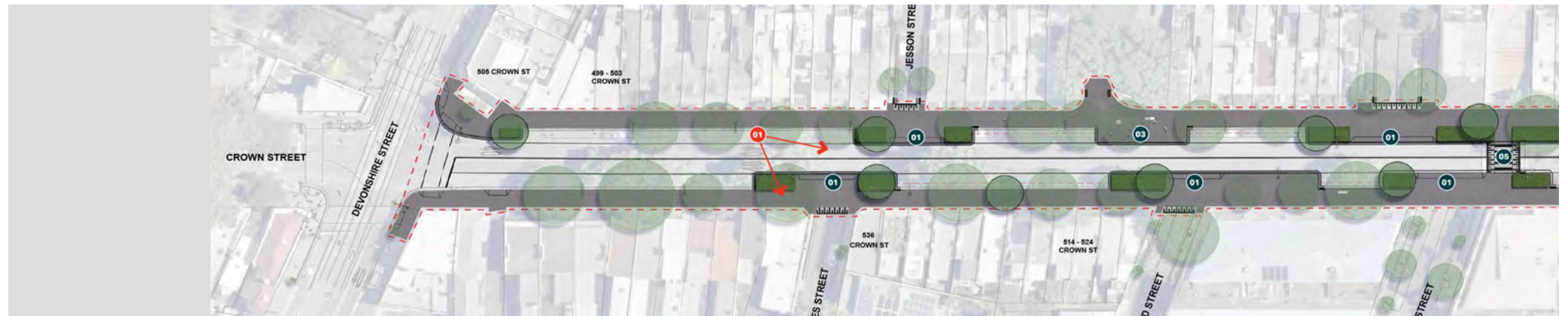


	PHASE 4 Devonshire St to Foveaux St			PHASE 3 Foveaux St to Albion St			PHASE 2 Albion St to Campbell St			PHASE 1 Campbell St to Oxford St		
	Existing	Current concept	Peer review concept	Existing	Current concept	Peer review concept	Existing	Current concept	Peer review concept	Existing	Current concept	Peer review concept
Pre-Covid car parking spaces on Crown St (minimum per Australian Standard)	62	n/a	n/a	23	n/a	n/a	46	n/a	n/a	20	n/a	n/a
Car parking spaces replaced by alfresco dining	-7	n/a	n/a	-12	n/a	n/a	-4	n/a	n/a	-4	n/a	n/a
Car parking spaces on Crown St (minimum per Australian Standard)	55	52	56	11	12	17	42	38	44	16	14	17
Car share spaces relocated from Crown St to side streets	n/a	0	4	n/a	0	1	n/a	0	0	n/a	0	1
Reduction in car parking spaces on side streets	n/a	0	-4	n/a	0	-1	n/a	0	0	n/a	0	-1
Street trees on Crown St	39	51	53	18	26	26	31	41	43	8	14	14

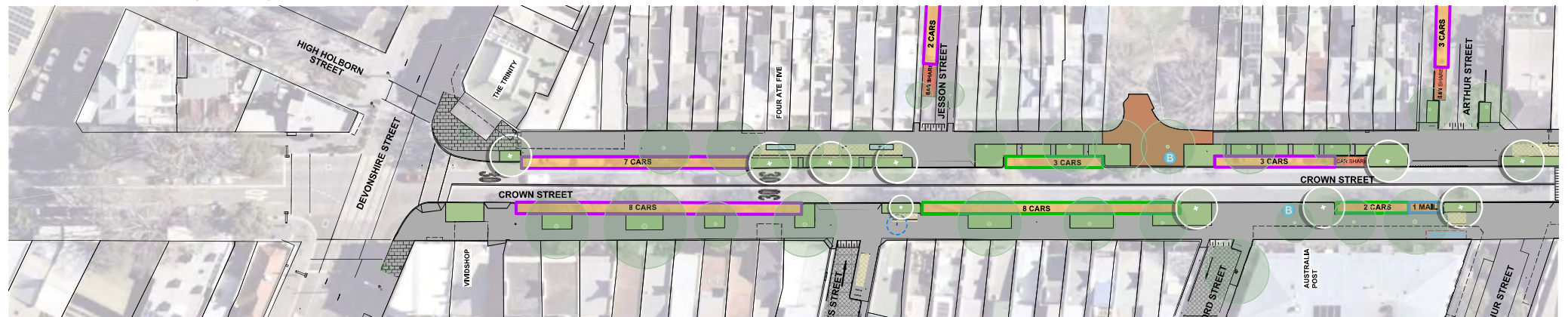
APPENDIX 1 – DESIGN COMPARISON PLANS



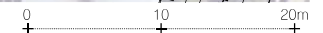
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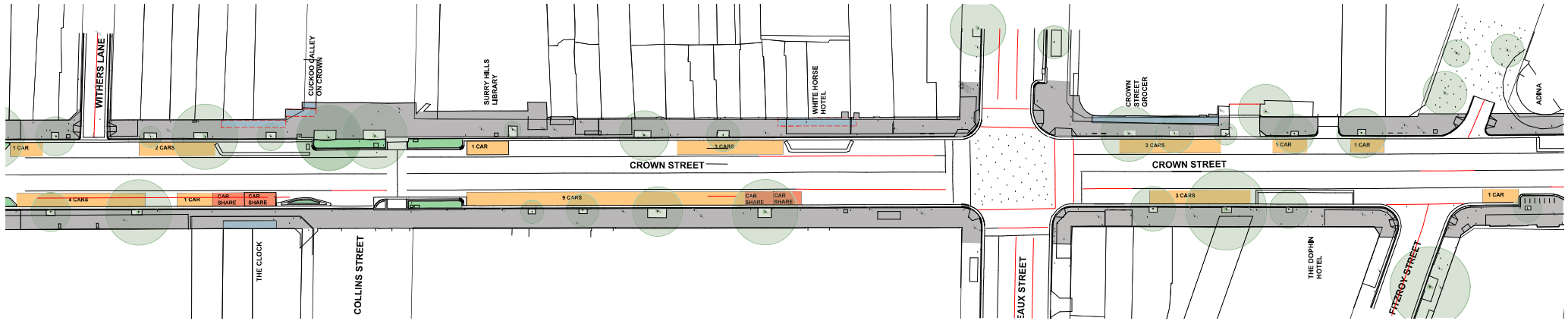


CURRENT CONCEPT (AECOM)



PRELIMINARY UPDATE (ASPECT STUDIOS)

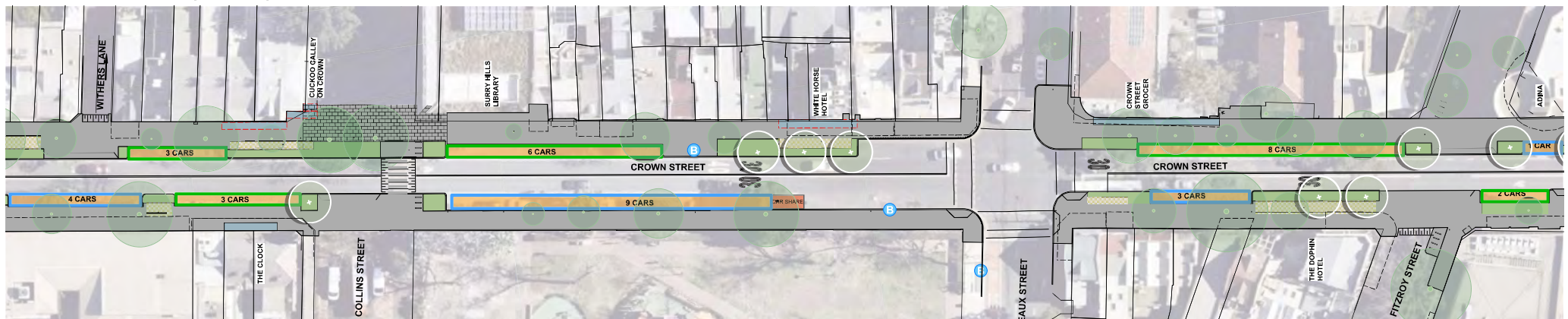




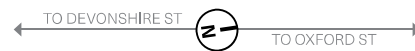
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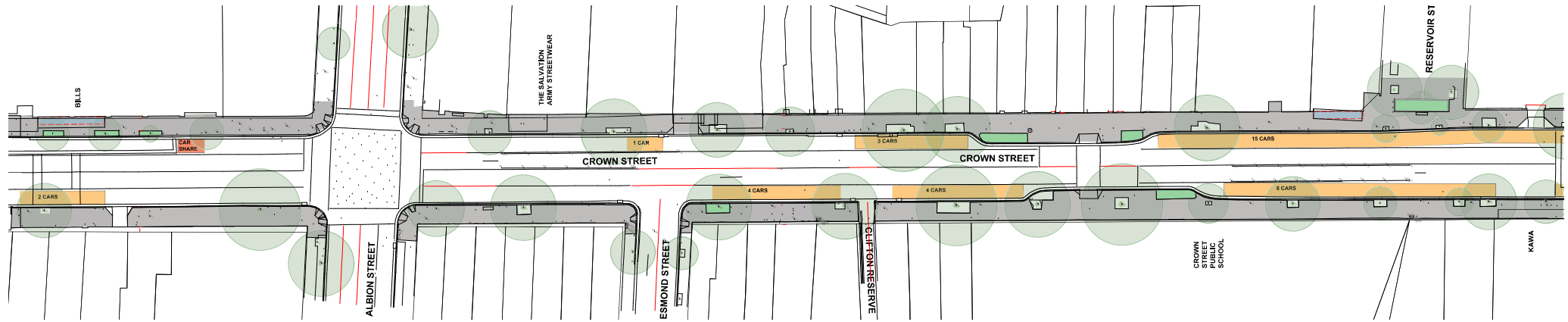


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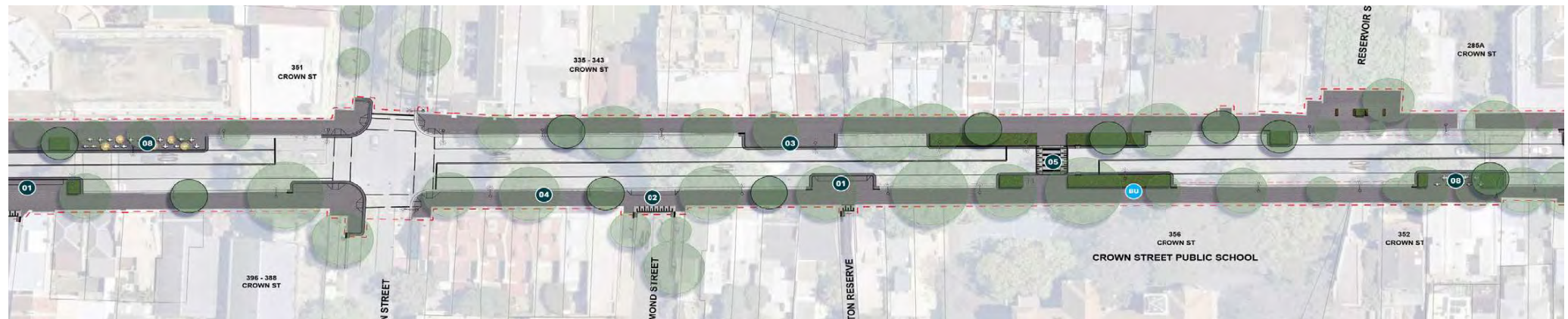


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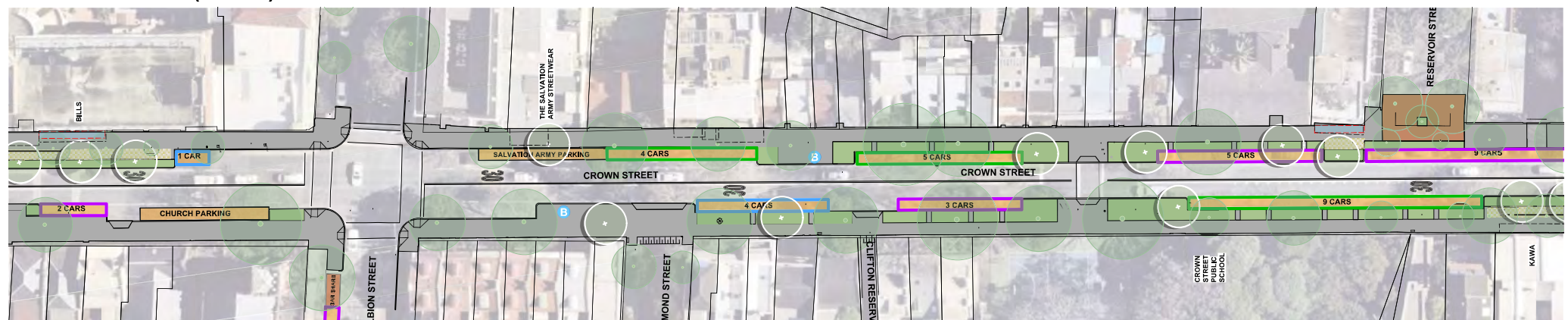




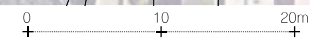
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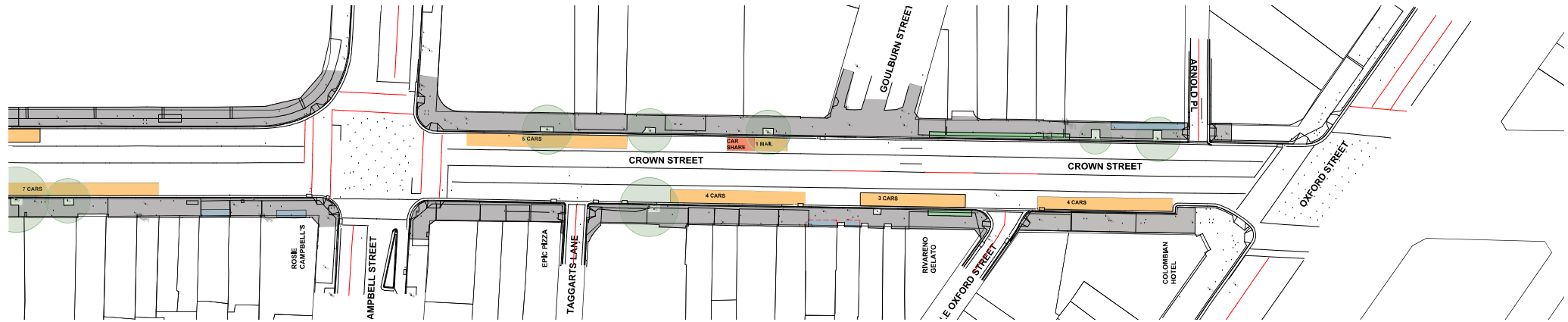


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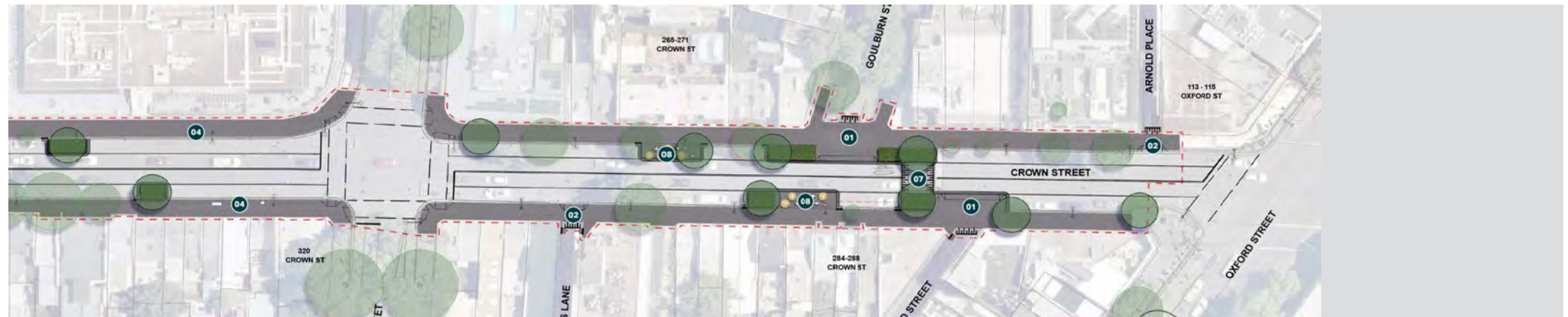


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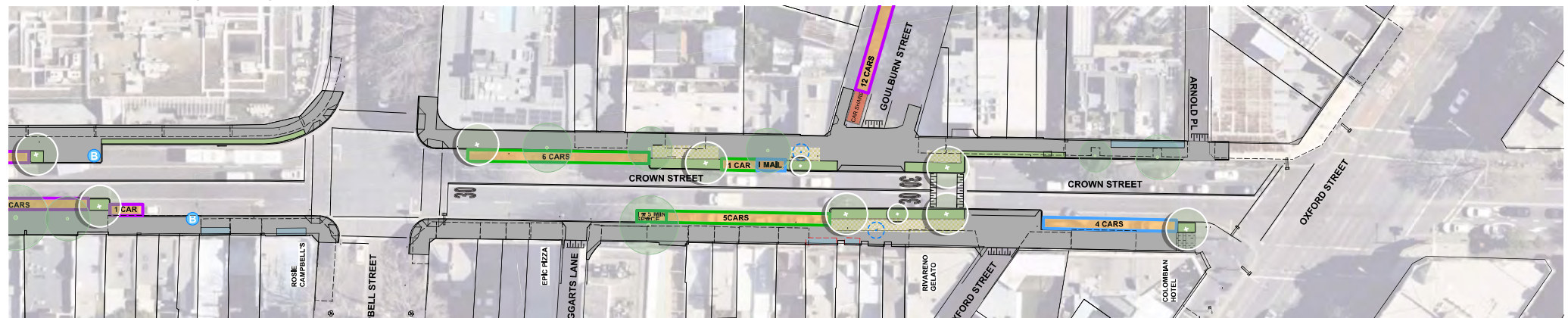




EXISTING



CURRENT CONCEPT (AECOM)



PRELIMINARY UPDATE (ASPECT STUDIOS)

